



# Palou Complete Street Project

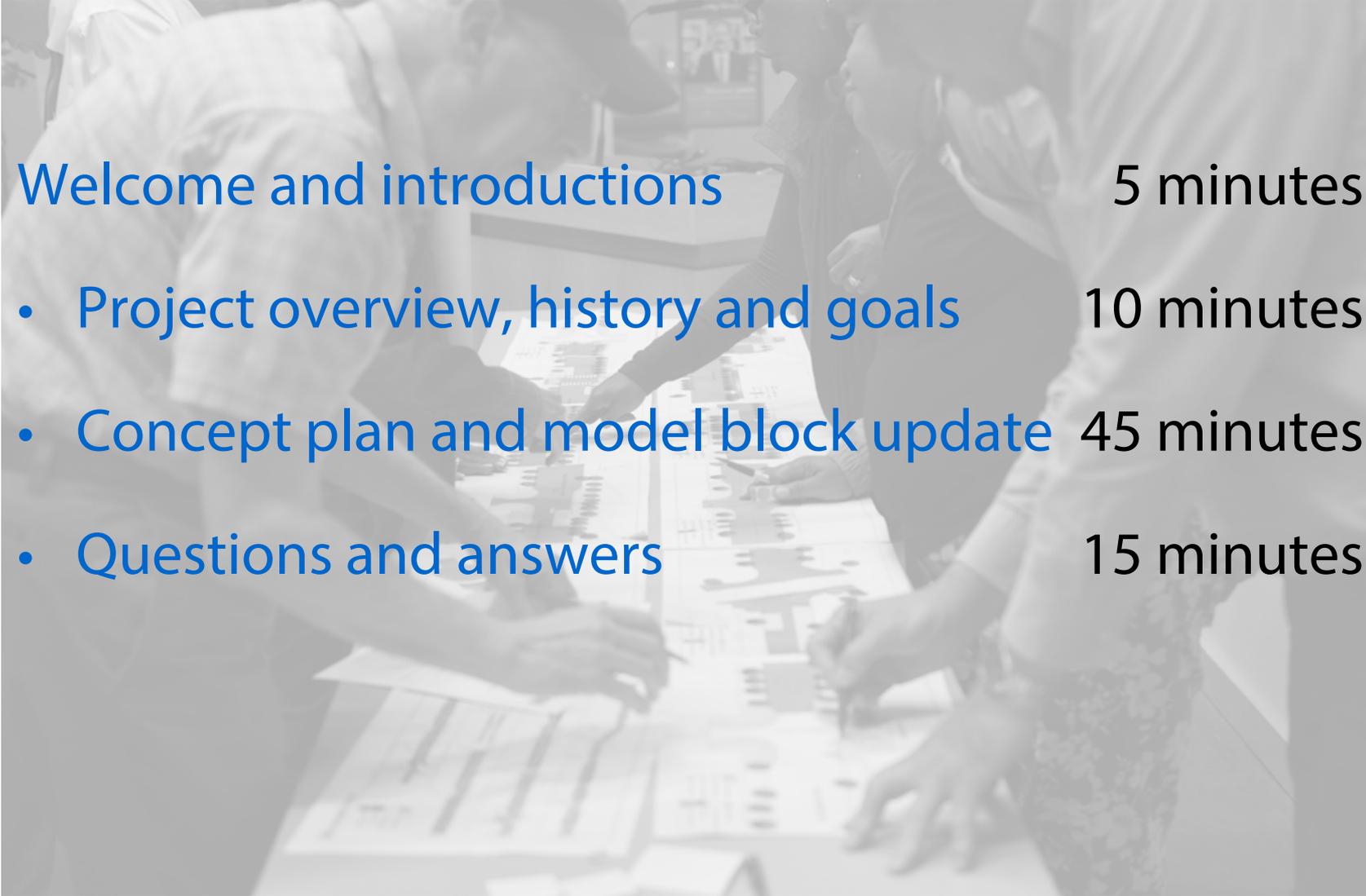
## Barneveld Avenue to Crisp Road

Project Concept Design

Community Meeting #3

July 15, 2015

# Agenda

- 
- Welcome and introductions 5 minutes
  - Project overview, history and goals 10 minutes
  - Concept plan and model block update 45 minutes
  - Questions and answers 15 minutes

# Project Team

## SAN FRANCISCO PUBLIC WORKS

**Oscar Gee**, Project Manager

**Marci Camacho**, Project Management Assistant

**Bo Sousa**, Streets and Highways

**Bill Bulkley**, Landscape Architect

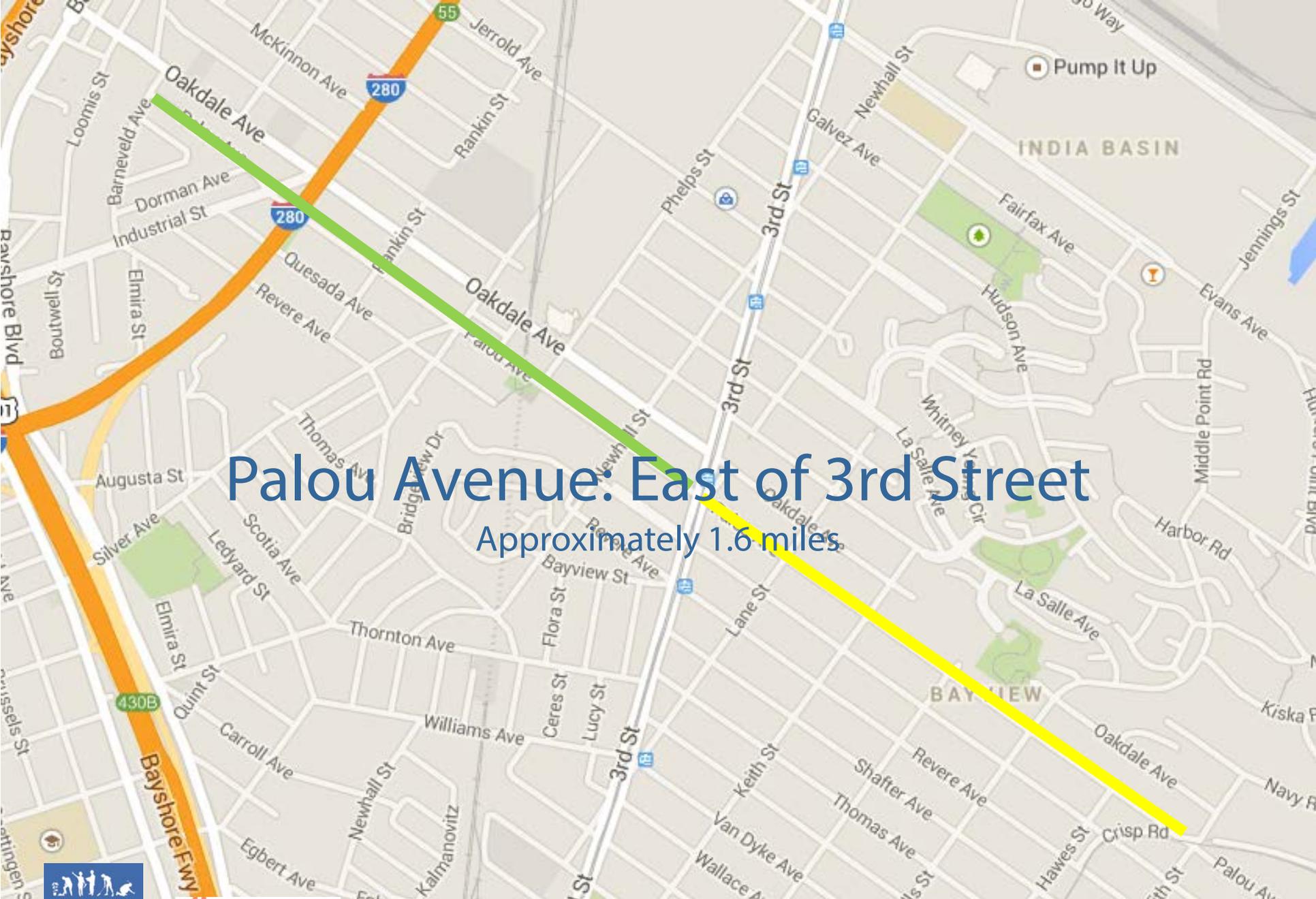
**Kevin Quach**, Landscape Architecture

**Julian Pham**, Public Affairs

## MUNICIPAL TRANSPORTATION AGENCY

**Felipe Robles**, Transportation Planner





Palou Avenue: East of 3rd Street  
Approximately 1.6 miles

# Schedule

Current – June 2015

July 2015 – Fall 2015

Spring 2016 – Spring 2017

planning phase

design phase

construction

# Overall Project Budget

Streetscape budget (2011 Prop B funds): **\$3.68 million**

Paving budget: **\$3.45 million**

Sewer budget: **\$1.9 million**



Improve pedestrian safety



Provide traffic calming



Greening

Review pedestrian safety, traffic calming, greening, and improved traffic times project goals with guiding policies from:

- Better Streets Plan
- Complete Streets Policy
- Bicycle Plan
- Vision Zero
- Muni Forward
- Walk first
- Green Connections
- Candlestick & Hunters Point Shipyard long term plan



## Project Goals

# Design Considerations

Improvements and amenities for people walking at intersections of: Quint-Silver, Dunshee, Phelps, Newhall, Keith, Jennings, Ingalls, and Hawes.

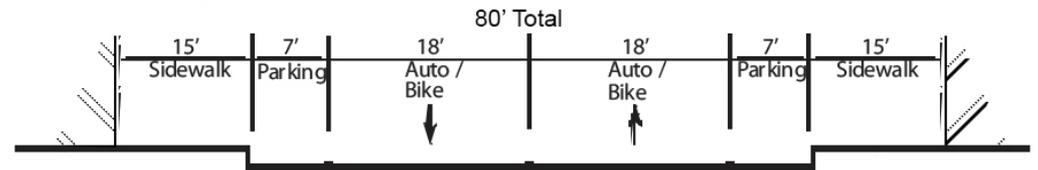
Improve visibility at all 4-way stops

Enhance bus shelters with “Next Bus” system

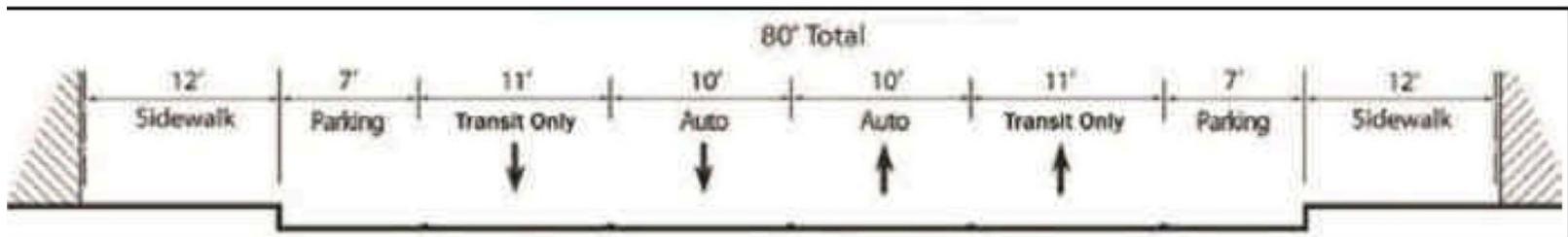
Streetscape improvements: Street trees and plantings

Review Candlestick Park-Hunter’s Point Shipyard and Bayview Transportation Improvement Plan from 3<sup>rd</sup> to Crisp.

# Candlestick Park-Hunter's Point Shipyard Bayview Transportation Improvement Plan Palou Avenue from 3<sup>rd</sup> to Crisp



Existing Palou street section



Proposed Palou street section



Repaving



Curb Ramps



Crossing Improvements



Landscaping



# Key Elements



Corner and bus bulbouts



# Design Elements

Crosswalk treatments



Joint utility pole



Bus stop improvements

Planting



Street trees

# Summary of Community Meeting #2 Feedback

## **Pedestrian safety, traffic and parking:**

1. No bike lanes on Palou – too narrow, too many buses
2. Slow traffic
3. Need traffic calming
4. Need parking on Palou: don't remove parking
5. Use speed bumps to reduce speed (Under review by SFMTA)
6. Get cars off sidewalks

## **Block and intersection specific comments:**

1. Specific comments by block / intersection were each taken into account by the designers. Please see website for community meeting questionnaire tally and workshop notes.
2. See Key Plans for notes keyed to each intersection

# Summary of Community Meeting #2 Feedback

## Community connection opportunities at:

1. Community garden and Phelps Mini Park
2. Vacant lot at Jennings
  - Model block ideas: amphitheater, Farmer's Market on Saturdays, Children's playground, mini-park, plaza- combination of use space.

## Trees and landscaping:

3. Trees and landscaping: greening
4. City should take care of trees

## Other:

1. Better bus shelters
2. Benches
3. Garbage cans at bus stops
4. Cleanliness

For community meeting presentations and notes, visit [sfpublicworks.org/palou](https://sfpublicworks.org/palou)

# Key Notes for Intersection Concept Design:

1. No change to parking
2. No room to add parking (SFMTA doesn't approve of new diagonal parking along bus routes)
3. Made bulbouts narrower to keep parking
4. Keeping existing diagonal parking
5. Removed bulbouts to keep parking
6. Made bulbouts narrower to keep street width
7. Removed bulbouts due to budget constraints
8. New required bus pads
9. Bus can make turning radius
10. Removed bulbouts as curb ramps are new
11. New curb ramps only
12. No room to add median islands
13. Removed median islands due to budget constraints

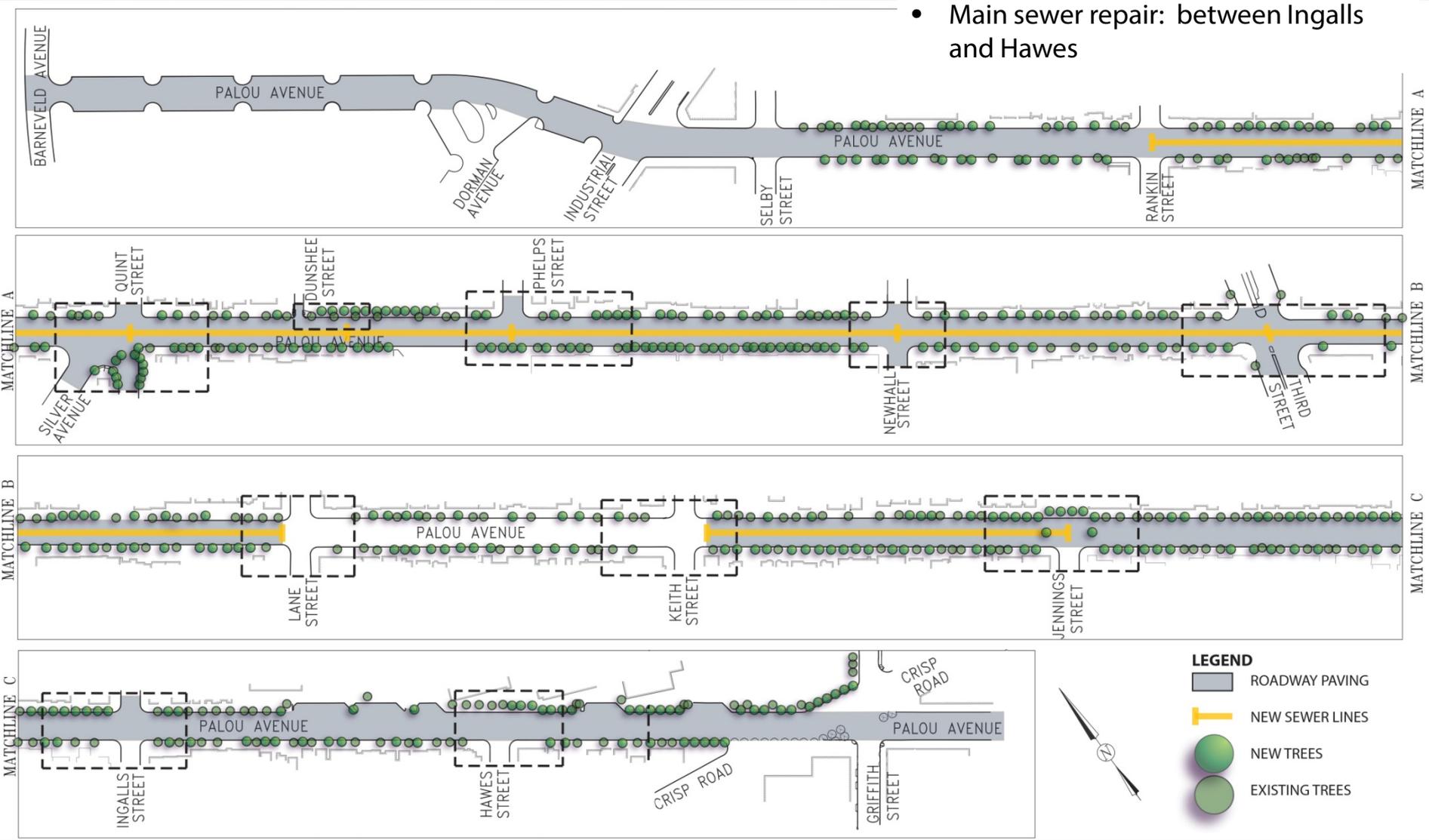
## Notes:

1. The Key Notes relate to the community comments for each intersection.
2. Review Key Notes with presentation slides.

# Key plan for intersection concepts

## Sewer work not shown on Key Plan:

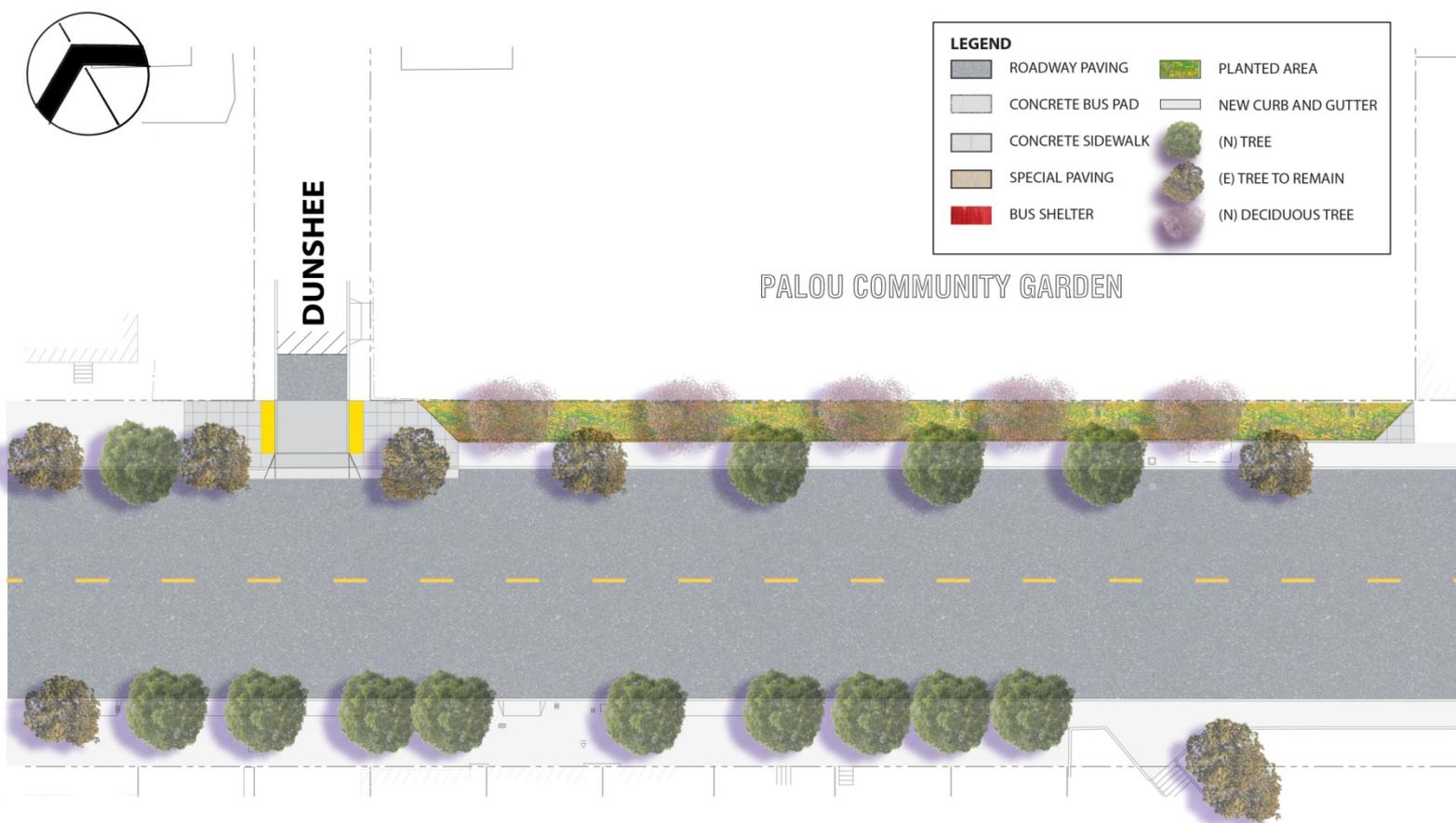
- Side sewer lateral replacement between Jennings and Ingalls
- Main sewer repair: between Ingalls and Hawes



# Quint – Silver

# Key Plan Notes: 1,2,8,9,13





# Phelps Street

# Key Plan Notes: 2,5,7,8

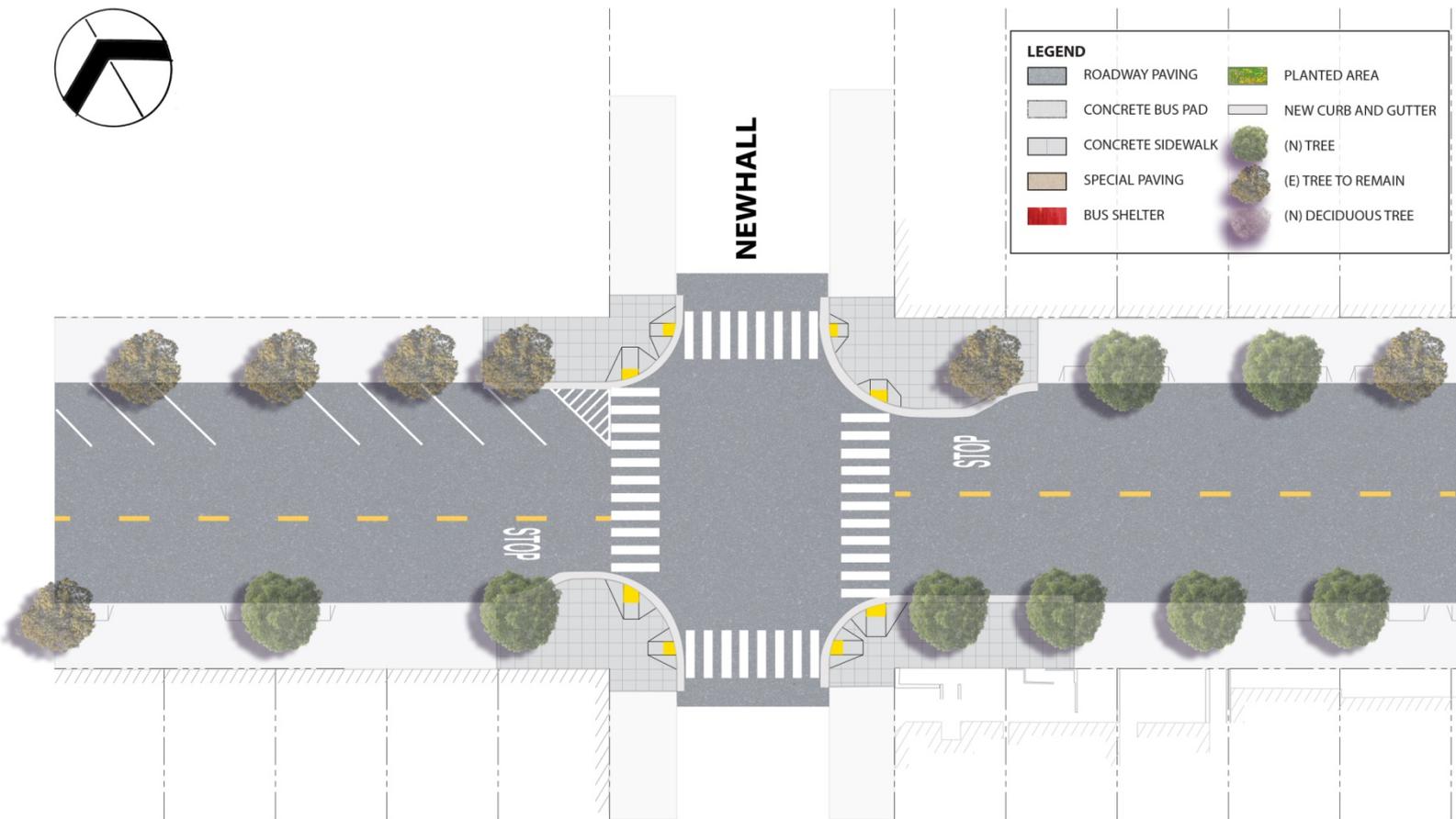


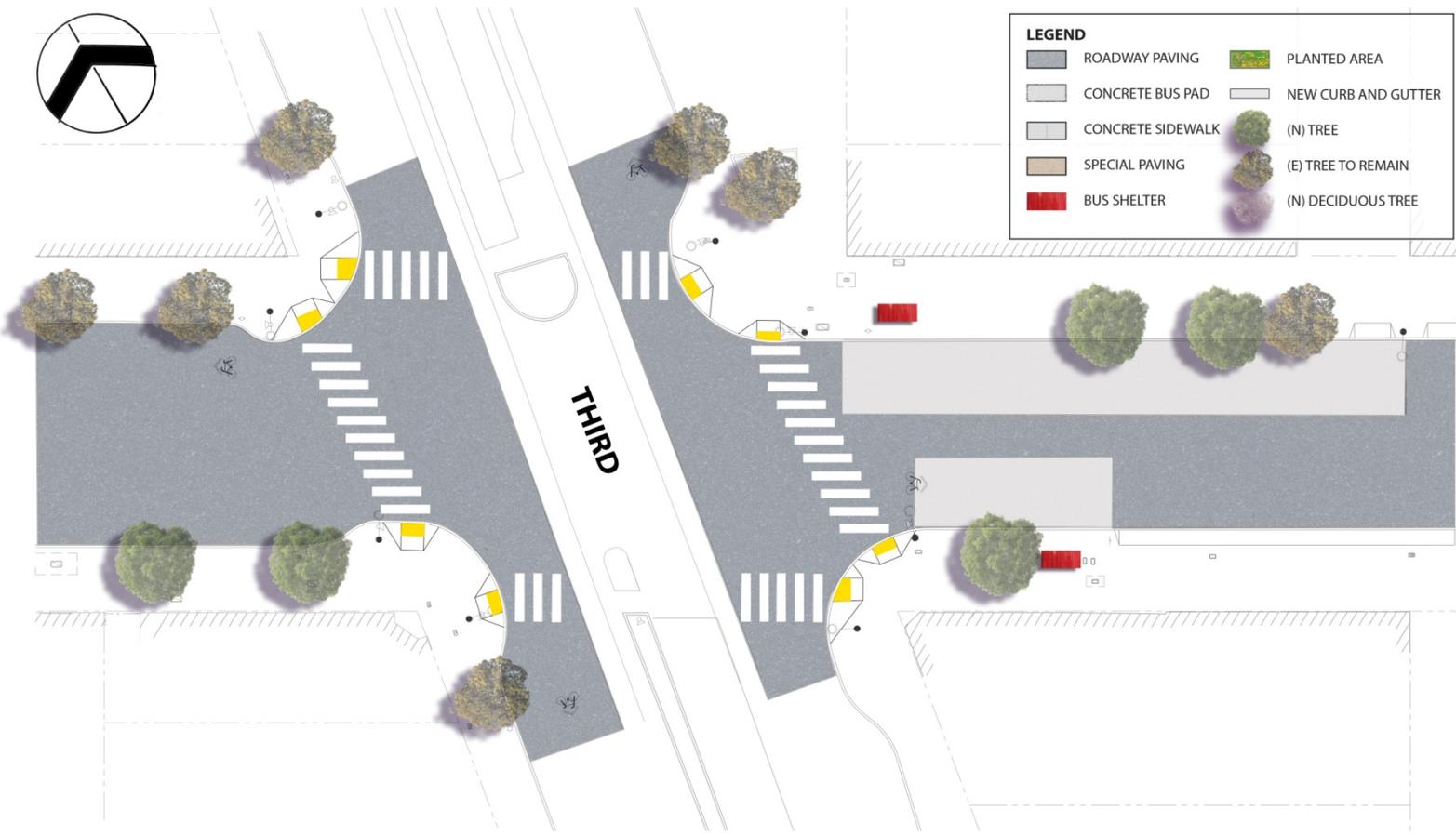
LEGEND	
ROADWAY PAVING	PLANTED AREA
CONCRETE BUS PAD	NEW CURB AND GUTTER
CONCRETE SIDEWALK	(N) TREE
SPECIAL PAVING	(E) TREE TO REMAIN
BUS SHELTER	(N) DECIDUOUS TREE



# Newhall Street

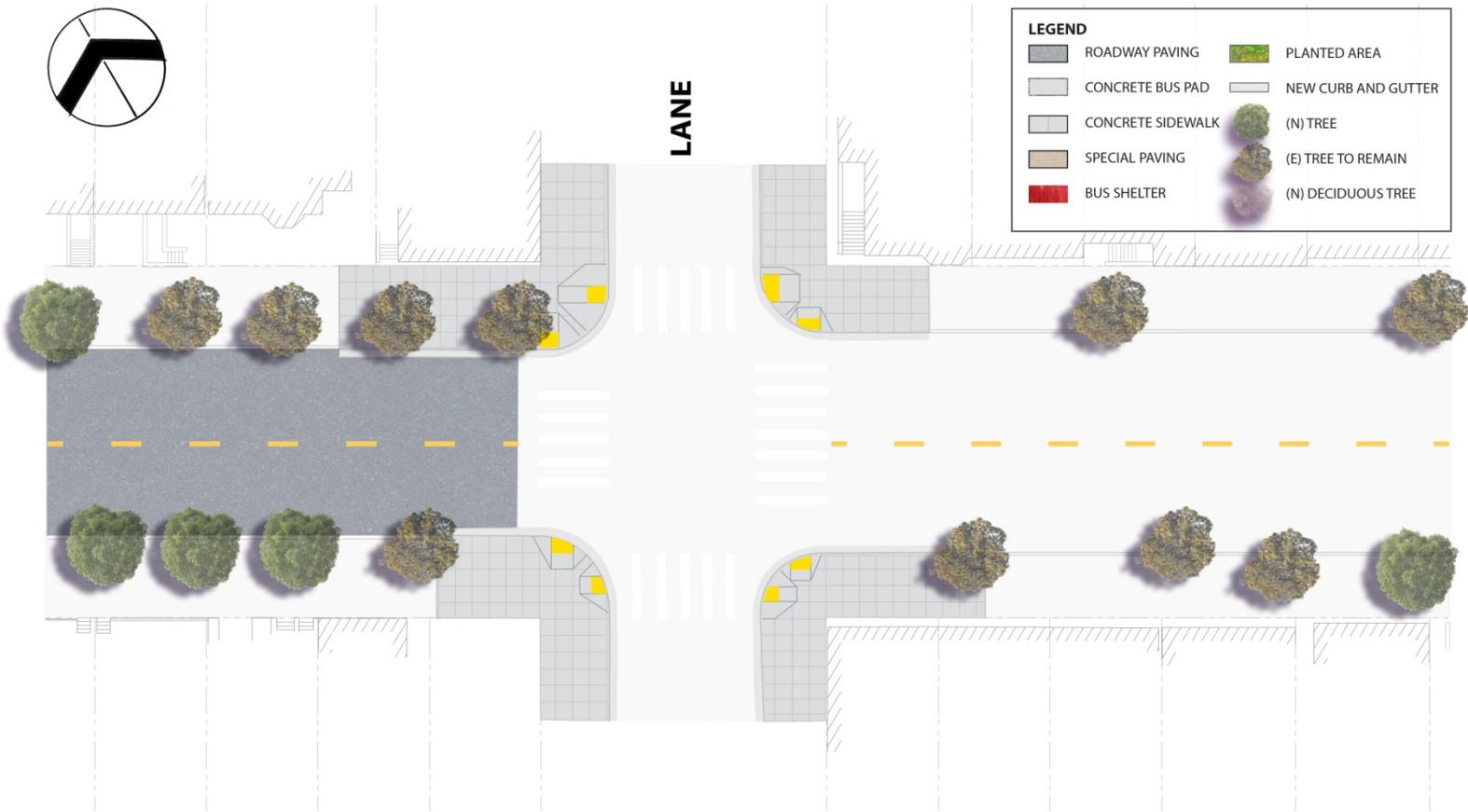
# Key Plan Notes: 4,5,6,7,9





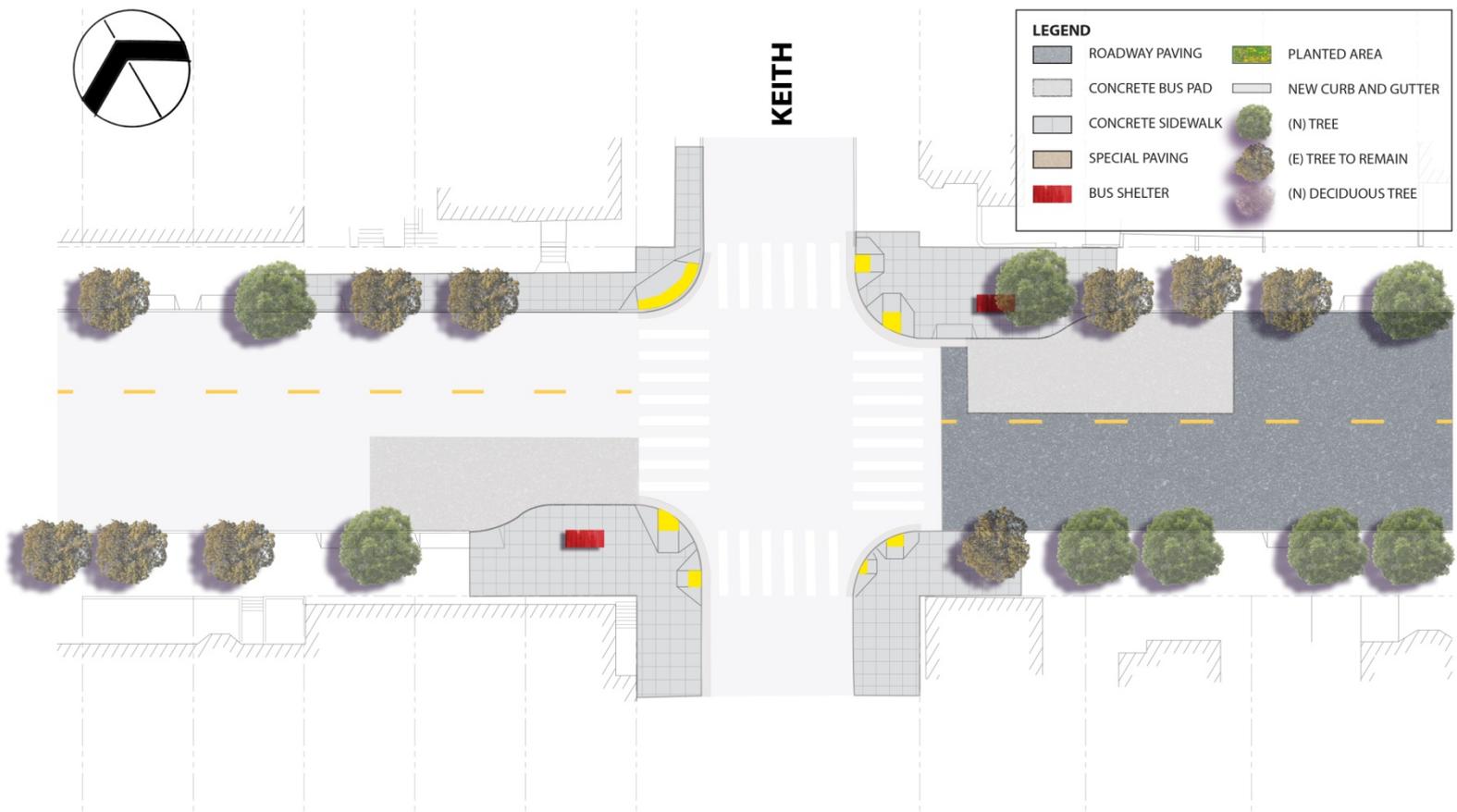
# Lane Street

# Key Plan Notes: 1,7,10



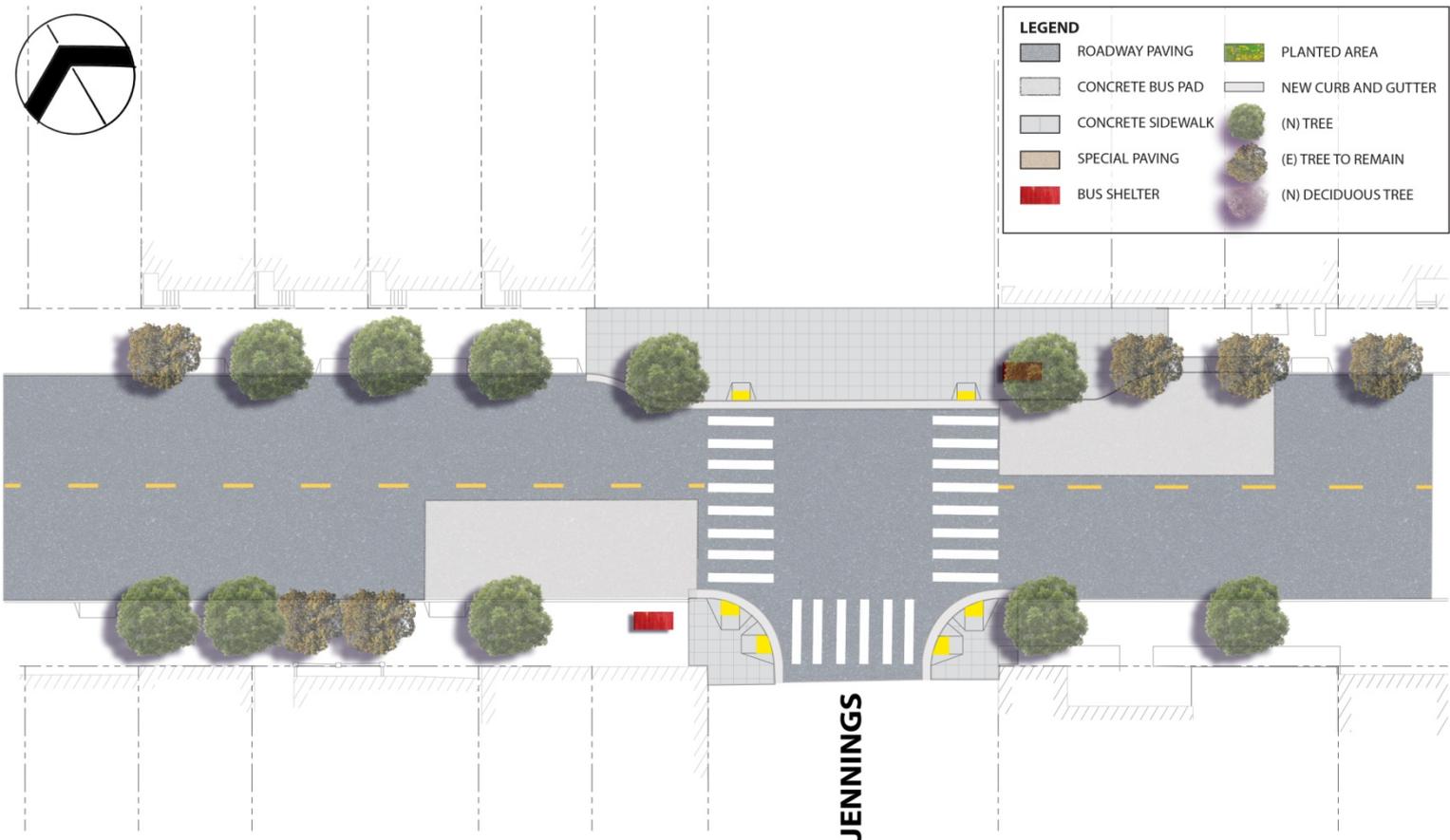
# Keith Street

# Key Plan Notes: 3,5,7,8,9



# Jennings Street

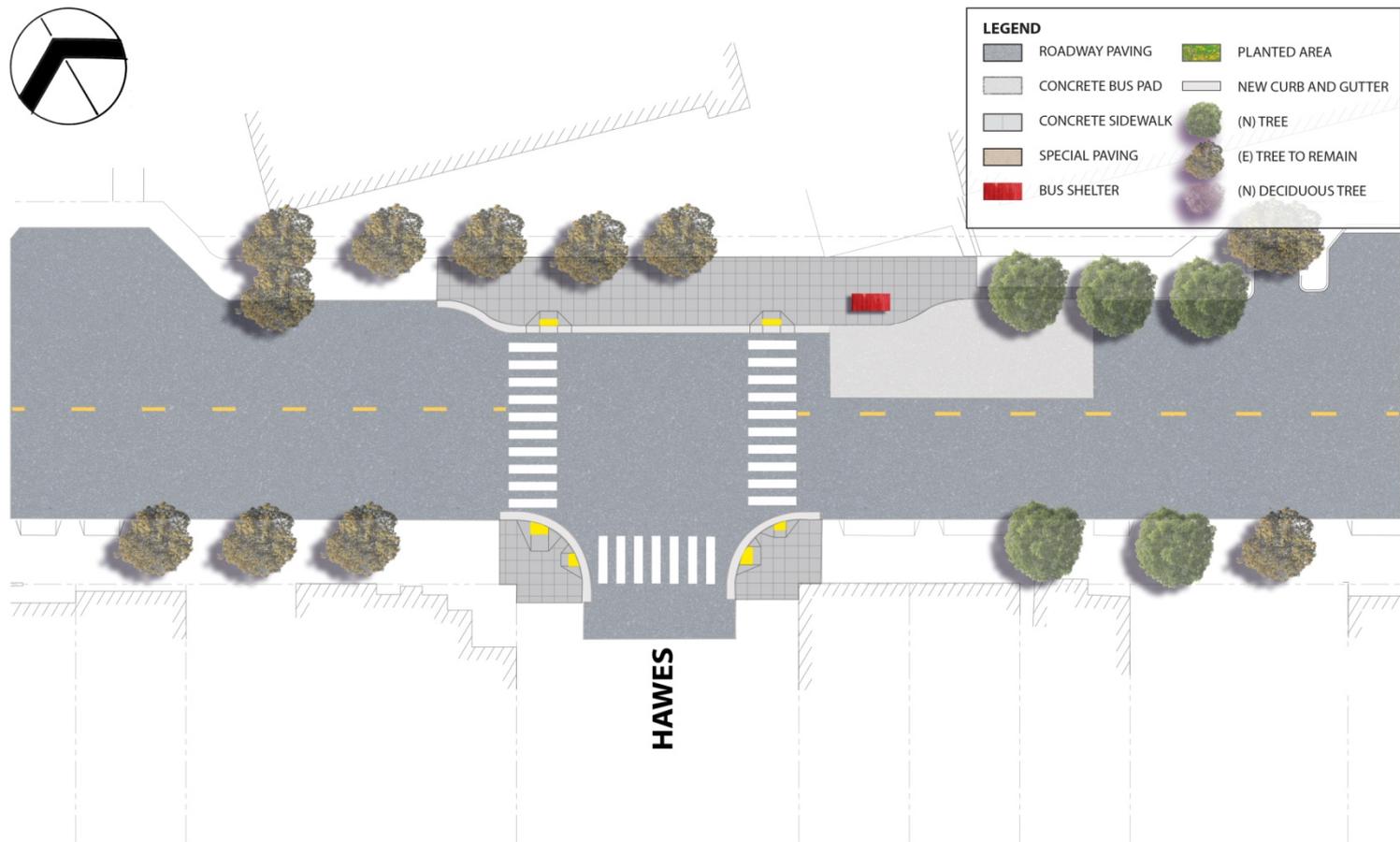
# Key Plan Notes: 5,8,9,11,13



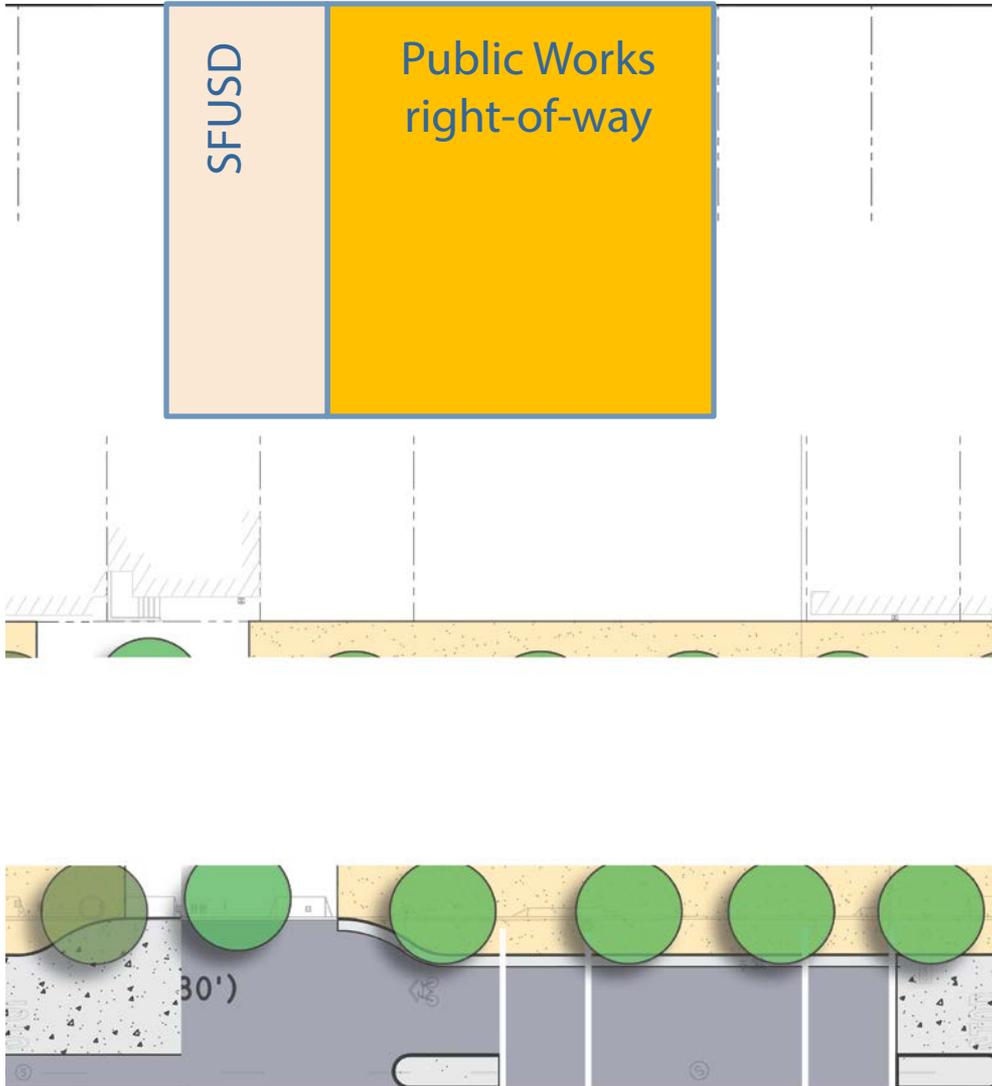


# Hawes Street

Key Plan Notes: 5,7,8,13



# Possible Model Block Opportunity: Jennings



**At both community meetings, the community expressed interest in vacant lots at Jennings:**

- Public Works right-of-way at Jennings
- San Francisco Unified School District parcel

**Community suggestions for vacant lot:**

Amphitheater, Farmer's Market on Saturdays, Children's playground, mini-park, plaza, and or combinations of use of the lot.

**Opportunities available:**

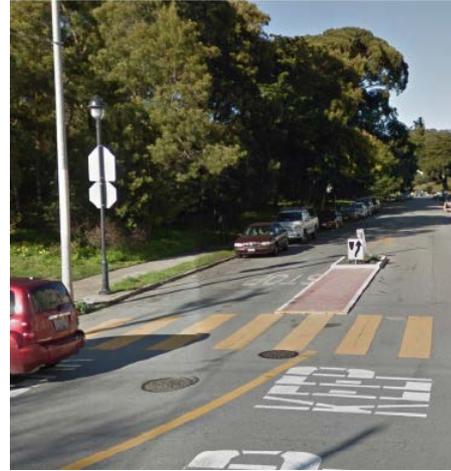
- OCII grant for \$450,000 construction funding – applied for Grant
- Challenge Grant for funding to help with model block project
- SFUSD as a community partner in programming, connecting the school to the greater neighborhood, and potentially for the additional parcel
- Foster community building and stewarding to develop ideas for the lot plan and coordinate with neighbors to make it happen

# Additional proposed work – pending funding:

If additional funding is acquired: median islands and bulbouts that were removed for budget purposes may possibly be added back into the design.



Painted island and thumbnail



Raised island with mountable section for driveway and emergency vehicle access



Raised island and painted thumbnail



Pedestrian bulb outs

## Next steps:

**August: Model Block Grant:** presentation of grant application to OCII Board will be made in August.

**Fall 2015:** Tree removal and planting notices will be posted on tree or mailed to adjacent property owners

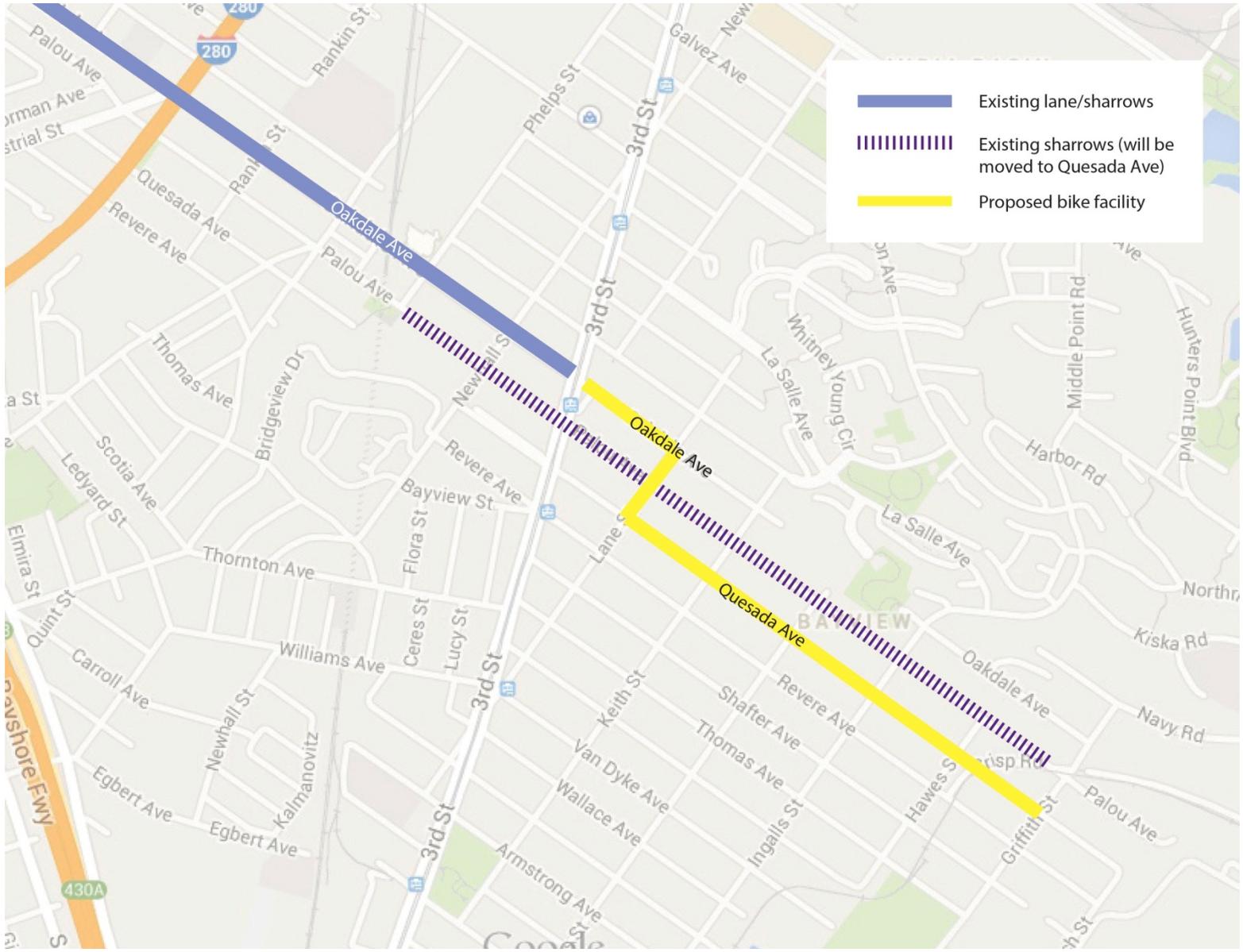
### **Street Light Update:**

The PUC will be embarking on a LED conversion project for all City owned poles starting summer of 2016:

- Silver to Keith Streets
- Joint utility poles (wood poles): not scheduled for conversion to LED at this time.

For project updates, visit [sfpublicworks.org/palou](http://sfpublicworks.org/palou)

# SFMTA Proposed Bike Route to Improve Safety



# SFMTA Proposed Bus Route

## 23 Monterey

**Faster, more direct trips through Bayview**  
**Safer, more comfortable streets for walking and waiting**

### PROPOSED CHANGES

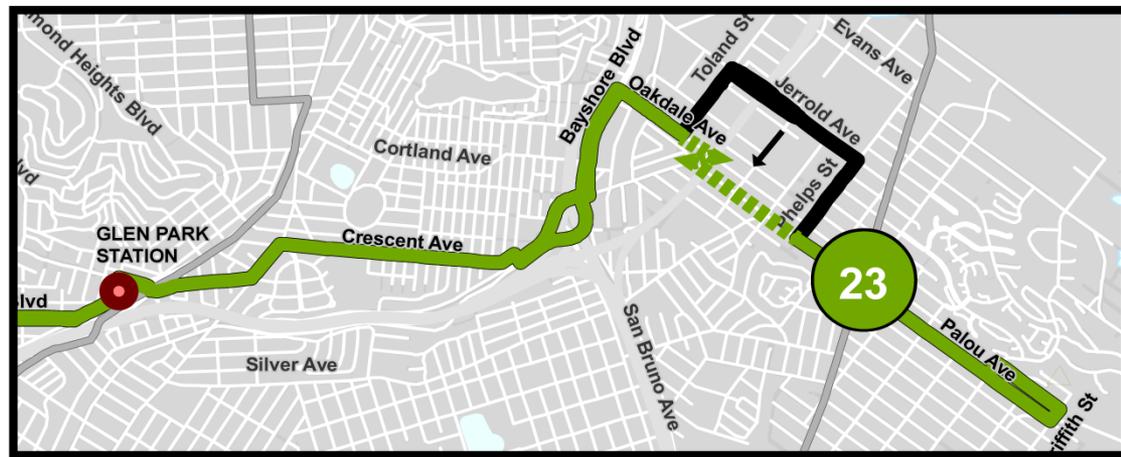
Reroute 23 Monterey 3 blocks south, off Jerrold Ave and onto Palou Ave

### EXPECTED OUTCOMES

**Reduce travel time** and improve reliability by minimizing twists and turns

**Increase ridership** by moving line closer to high-demand residential areas, and away from low-demand industrial areas

**Improve the walking, waiting, and riding experience** for customers by taking advantage of streetscape improvements along Palou Ave



### BY THE NUMBERS

**900**

Average weekday boardings between Griffith St and Bayshore Blvd

**10%**

Percent of Bayview customers who use a Jerrold Ave stop

**20**

Typical travel time between Griffith St and Bayshore Blvd

**5**

Minutes required to serve the Jerrold Ave detour

The Jerrold Ave detour adds over 5 minutes through Bayview (25% of travel time), but serves only 10% of Bayview customers



# Title VI of the Civil Rights Act of 1964 states:

- Title VI prohibits discrimination on the basis of race, color, and national origin in programs and activities receiving federal financial assistance.
- San Francisco Public Works wants to ensure that residents and communities impacted by this project have been included in the decision making process.
- Participation in the Title VI questionnaire assists SFPW in assuring that we have an effective and representative community outreach program

# Questions?

Thank you for participating tonight!

